

REVISED STANDARD DRAWING #H-1011

Construction Advisory Council Meeting

January 27, 2021



PRESENTATION CONTENTS

- Highlights
- General Notes
- Case Side Treatments
- Corner Cases
- Midblock Cases
- Island Cases
- Temporary Cases
- Miscellaneous Details & Examples
- D.W.S.



H-1011: PEDESTRIAN RAMP OVERVIEW

- In compliance with ADA 2010 and PROWAG 2011
- New standard cases:
 - Five corner cases: C1, C2, C3, C4, and C5
 - Two midblock cases: M1, M2
 - Two island/median cases: IM1, IM2
 - Three temporary cases: T1, T2, T3
- Detectable Warning Surface (D.W.S.) requirements
- Curb type varies
- Steel faced curb at ramps now in new standard detail H-1060
- Sidewalk Curb also in new standard detail H-1060
- New side treatment options

LEGEND

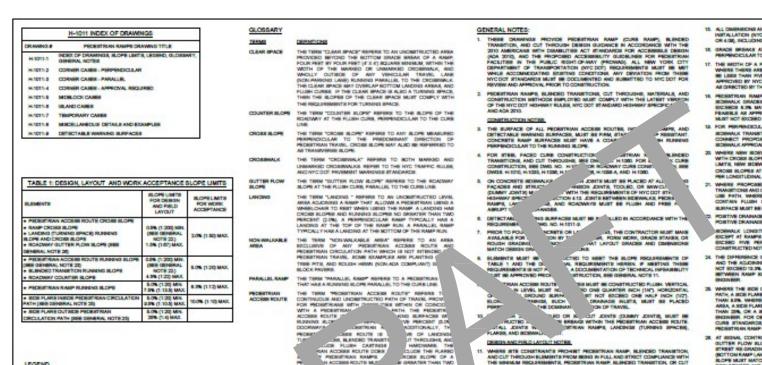
LIMIT OF TIMOH

DETAIL INDICATOR

SLOPE ASSOCIA

CHECKED BY:

CONCRETE SEGMENT



MIDDEL DRAWING &

SHREE TO A THE PLE THE MAY CON APPHALT BLOCK PAYERS, A **BLURGITONE** STATE OF STANDARD HIDWAY SPECIFICATION BOY, BOD, BOK, OR OTHER PREPARED TONS 4.TE FOR EXAMPLE, THE PEDESTROAN NOT INCLUDE TREE PITS, PLANTING NO BURN ATION PAR CHARLE ARMAE THE PECKETRIAN DRE THE PLANED MORE OF OR COHER

PEDERTRIAN

PERPENDICULAR

PLEMENT BLOPE

TURNING SPACE

SCH FLAS

AN RAMPS. PERPENDICU HAR A RUNNING BLOPP THAT IS APPRICEMATING

SALAR TO THE CLIRE LINE TUNNING SLOPE REPERK TO ANY SLOPE MEASURED. RUNNING SLOPE MAY ALSO BE REPERRED TO AN

LIDINAL BLOPS E TERM TROS PLANS" REPERS TO THE PLANSO ROS OF A PERPENDICULAR RIMP, WHOSE SLOPE IS MEASURED PARALLIS. TO THE CURR LINE.

THE TERM TEDEWALK TRANSFOR RAMP REFERR TO A RAMP REPORTS FARALIE. TO THE CLERK LINE TRANSPICIONS BETWEEN DEFERRANT RESIDENCE GRADER THE BLOPE OF A RESIDENCE TRANSPORT OF SMAP IS DESCRIBE THAN DUE PRECING (S.DN) AND IS NOT DISSATES THAN 8.7%.

> THE TERM TURNING SPACE" REFERS TO AN UNDESTRUCTED LEVEL AREA IN A PROBETRIAN ACCRES ROUTE THAT PROVIDES REPROBRE SPACE TO ALLOWS A PROBETRIAN LIBROR A WHOSLOWIN TO PROFORM A TURNING MOVIMENT TURNING SPACES MAY DYSPLAP OTHER AREAS WITHIN THE PROBETRIAN ACCISIOS ROLITE. BLICH AS CLEAR SPACES AND LANDINGS. THE CROSS SLOPES AND RUSHING SLOPES OF A TURNING SPACE MUST BE NO GREATER THAN TWO PERCENT (LON).

AND CUT THROUGH BURNINTS FROM BRING IN FULL AND STREET COMPLIANCS WITH THE MINISTER REQUIREMENTS, PROSETTION RAMP BURNING TRANSPORT, OR CUT THE THE WAR AND THE OWN COMMON THE PROPERTY AND THE PARTY BARRIE ANY PEDBITRIAN RAMP, ILENCHO TRANSITION, OR CLIT THROUGH THA IS NOT IN PULL AND STRICT COMPLIANCE WITH THE MINISTER REQUIREMENTS. MILET BE DECLIMENTED BY THE BROWNER, ARCHITECT, OR LANGICALS ARCHITECT, AND APPROVED BY MYC DOT OR NYC DDC AS SEINS TRONSCAUL INFRAME.S.

12. LANSING ITURNING SPACE REQUIREMENTS:

- 121. A LANSING ITURNING SPACES MUST BE PROVIDED AT THE TOP OF PERPENDICULAR RAMPS AND BURNOSD TRANSPICING, AND AT THE BOTTOM OF PARALLIS, RAMPS
- LANCHER MAY CHERLAP WITH ADJACENT LANCHER
- WHERE MILTIPLE LANDINGS ARE SEPARATED BY A DISTANCE LESS THAN TWO PEET (7), THEY MUST BE COMBINED INTO A SHARED LANCING.
- A SINGLE LANGING MAY SERVE MORE THAN ONE RAMP OR BLENCHD TRANSITION.
- TO A LANDING MUST NOT OVERLAP A NAME OF BURNORS TRANSPORT.
- THE A LANCING CONSTRAINED BETWEEN A PROSETRIAN RAWP OR ILLENCED. TRANSPORM AND AN OBSTRACE MUSE HAVE A MINERUM LENGTH OF FINE FIRST (IT) RETWEEN THE RAMP OR BLENDED TRANSPORM AND OBSTRACE. A LANCENG THAT IS UNCONSTRAINED MUST HAVE A MINIMA LINEITH OF FOUR PRIST US.
- 127. A LANCING MUST BE PROMOBED FOR THE FULL WIDTH OF A PEDBITRIAN RAMP OR BLUMOED TRANSITION, EXCEPT WHERE INDICATED, BUT NOT LIKE THAN
- 13. WHERE FEMILE, THE RUMING SLOPE OF PERPENDICULAR RAMPS SHOULD ALKIN PARALLEL WITH THE MARKED OR LINEWAKED CROSSWILLS. IF A RAMP IS NOT ALIGNED WITH THE MAKKED OR LIMMARKED CROSSWALK, THE CLEAR SPACE MUST DIRECTION OF THE WAXBELIN RUNNING AND CROSS BLOPES
- 16. AT MARKED CROSSWALKS, PEDESTRIAN RAMPS (EXCLUDING PLANSS) AND CLIT THEOLOGIES MUST BE LOCATED WITHIN THE CROSSWALK MARKEDS. REYORD THE PROSESTIONS REMAIN SICTION SHOULD REPORT AND ARREST MY POLY FIRST IF IT IS IT IS SUITED CLARK SPACE MUST BE RENOVED WITHIN MARKED OR UNMARKED. CRORINALIZA

ORDER PROPERTY.

15. ALL DIMENSIONS AND NOTES WILL BE APPLICABLE TO MY PEDESTRAN RAWP CLIES NETALLATER INTO DOT STANDARD HIGHWAY SPECIFICATION SECTION 4.07 4.08 OR 409, INCLUDING INTROPAL CURB AND OUTTER.

- TE GRADE BREAKS AT THE TOP AND BOTTOM OF PEDESTRIAN NAMES MUST BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN.
- 17. THE WIOTH OF A PECENTRIAN RAMP SHOULD NOT BE LESS THAN RIVE FEET (II) WHERE THERE ARE SITE CONSTRAINTS, THE WIDTH OF A PEDESTRIAN RAWP MAY SE LIBER THAN PIVE PIRIT (IT) BUT MUST NOT SE LIBER THAN POUR PIRIT (IT), AS APPROVED BY NYC DOT, PROBITTION RAMPS MAY SE WORK THAN PIVE PIRIT (IT), ALC:RECTED BY THE ENGINEER.
- TR. PEDESTRIAN RAMP LISHOTH MAY BE LIMITED TO 15 PEET TO AVOID CHAN ROBBIALY CRACKE A 15 FOOT LONG PECENTRIAN RAMP WITH A RLOPE THAT ROCKEDS BUTK MAY BE CONSIDERED CONSTRUCTED TO THE MACHINE SITENT PEAUERS AS APPROVED ONE NOTE TIL THE MAXIMUM RISE OF ANY RAWP RUM MUST NOT ROCKED SO NOTHIN
- 19. FOR PERPENDICULAR RAMPS AND BLENDED TRANSPICHE, 5 FEET TO 16 FEET BORNALY TRANSPOR RAMPS PARALLEL TO THE CURE LINE MAY BE LISTO TO COMMECT PROPOSED RAMP LANGINGS AND TURNING SPACES WITH STIRTING ROBWALK APPROACHES, AS DIRECTED BY THE ENGINEER
- 25. WHERE NEW ROBWALK OR RAMP CONSTRUCTION ADJOING EXISTING ROBWALK WITH CROSS SLOPES THAT EXCEED ADA COMPLIANT DESIGN AND CONSTRUCTION LIMITS, NEW ROBWALK CROSS ILLOPES CAN TRANSITION TO EXISTING ROBWALK CROSS SLOPES AT A SATE OF CHANGE SO DESIATES THAN ONE PERCENT CLOSE FER LONGITUDINAL FOOT, ARDINECTED BY THE BND MEER.
- WHERE PROPOSED ON BHARED USE PATHS, PEDESTRIAN RAMPS, BLENDED TRANSPORM AND OUT THROUGH BLIST EXTEND THE FULL WICTH OF THE SHARED USE FATH WHERE SCHOOLS NAMES WITHIN PROSETTION CROSSATION FATHER COMPANY FLUSH CLASSE AT STREET CROSSINGS. A DETECTABLE WARRING BURFACE MUST BE INSTALLED
- PORTIVE CRANAGE NUCE BE PROVIDED. RAMP CONSTRUCTION MUST NOT HINDER POSITIVE DEADAGE
- ROBWALK LONGITUDINAL SLOPES MUST NOT EXCERD FIVE PERCENT GUNG ECCEPT AT RAMPS AND RAMP PLANSE WHERE LONGITUDINAL ROADWAY PROFILE ROCHED FIVE PERCENT (SON), RIDWALK LONGITUDINAL SLOPES MUST SE CONSTRUCTED NOT TO EXCERD THE ROADWAY BLOKE
- IN THE DIFFERENCE IN SLOPE BETWEEN THE SOTTOM OF A PERFENDICULAR RAMP AND THE ADJORNED ROADWAY COUNTER SLOPE (CROSS SLOPE) SURFACE MUST NOT SOCIED TUDY. A MERILAN THAT FOOT OF LONG LINKS, SCHWARK, MAY SHI MEN SETWIEN RAMP SLOPE AND ROADWAY COUNTER SLOPE, AS DESCRID SY THE
- 25. WHERE THE ROLE OF A PROPERTIES HAVE ACCOUNT A PROPERTIES OFFICE ATION PATH, A SIDE PLANE MUST BE DESIGNED AND LAD OUT WITH A SLOPE NO GREATE THAN BUY, WHERE THE ROS OF A PROSETRIAN RAMP ADJOINT A NON-WALKARLE AREA, A RESIDENCIARE MAY BE DERIGARD AND LAD OUT WITH A RLOPE NO GREATER THAN 30%, OR A SEMANLY CLIES SAY SE CONSTRUCTED, AS DESCRIP BY THE SHOWER FOR OBJECTS IN THE FLATS, SEE DWG NO. H 1011 S. FOR SEEWALK CURS STANDARDS, SEE DWG. NO. H100. FOR SPECIFIC OPTIONS FOR EACH PEDESTRIAN RAMP CARS, SEE THE SIDE TREATMENT DETAILS FOR EACH CASS.
- AT RIGHAL CONTROLLED OR UNCOMPROLLED CROSSINGS WHERE THE SOUTHO OUT THE PLOW SLOPE INDIADWAY PROPILE RECEIP TWO PERCENT (CON) AND STREET BE CRADED IS NOT FRANCE. THE STREWN IS BORE AT THE PLUSH CLERK SCITCH FAMP LANDING OR RAIP CROSS SLOPE, PARKLE, TO THE SLITTER FLOW SLOPE MUST MATCH THE SOSTING GUTTER FLOW SLOPE (SCADWAY PROPER), AS DOCUMENTED AND APPROVED BY THE BYOINERS.

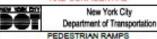
CASE SELECTION NOTES.

Appro

DATE

- FOR CORNERS WHERE THE PRODUCTION ORGALIZATION PATH IS BOTH FRET (F).
 WHERE OR SIGNATINE, USE PROPRIEDCULAR CASE OF OR C2, DWG. NO. H 10112.
 WHERE AND LEWOTH ALCOHOLOUPS RETURN IS LIMITED, USE CASE OR, DWG. NO. HITCH'S, AR DIRECTED BY THE BACKNESS.
- 28. FOR CORNERS WHERE THE PEDESTRIAN CIRCULATION PAIN IS LIKE THAN BONT ALONG CURR RETURN IN LIMITED, LIKE CARE CA. DWG. NO. H-1071-4, AR DIRECTED
- 29. FOR RAMPS NOT LOCATED AT A RIDEWALK CORNER, USE CASE M1 OR NO, DWO, NO.
- 35. FOR BLANCK WITHIN PROBRIEGAY CROSSINGS, USE BLAND CARRIE CARE INT OR
- ST. FOR TEMPORARY RAMPS, AND RAMP REQUIREMENTS IN CONSTRUCTION WORK JONES, INSETEMPORARY CASES CASE T1, T2, OR T3, DWG, NO. H-1071 F.

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Fingheer officent of Transportation	Associate Commissioner Infrastructura/Design Department of Design + Construction			
west	Scale	Drawing # H-1011-1		

INDEX OF DRAWINGS, SLOPE LIMITS, LEGEND,

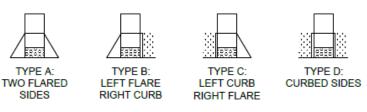
H-1011 GENERAL NOTES

- Slope Limits
- Glossary
- General Notes
 - Construction Notes
 - Design and Field Layout Notes
 - Case Selection Notes
 - Including: technical infeasibilities, construction inspection

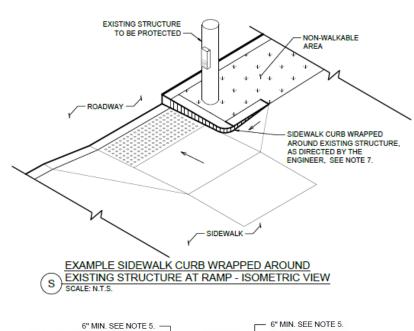
TABLE 1: DESIGN, LAYOUT AND WORK ACCEPTANCE SLOPE LIMITS			
ELEMENTS	SLOPE LIMITS FOR DESIGN AND FIELD LAYOUT	SLOPE LIMITS FOR WORK ACCEPTANCE	
PEDESTRIAN ACCESS ROUTE CROSS SLOPE RAMP CROSS SLOPE LANDING (TURNING SPACE) RUNNING SLOPE AND CROSS SLOPE ROADWAY GUTTER FLOW SLOPE (SEE GENERAL NOTE 26)	0.5% (1. \ \ 0.0) MIN. (SEE GE) ERAL NOTE 2) (.5% (1:67) MAX.	2.0% (1:50) MAX.	
PEDESTRIAN ACCESS ROUTE PUNNING SI UPE (SEE GENERAL NOTE 23) BLENDED TRANSITION TUNIN NG LOPE ROADWAY COUNTER LOPE	0.5% (1:200) MIN. (SEE GENERAL NOTE 22.) 4.5% (1:22) MAX.	5.0% (1:20) MAX.	
PEDESTRIAN RAMP RUNI 'NO SLOPE	5.0% (1:20) MIN. 7.5% (1:13.5) MAX.	8.3% (1:12) MAX.	
SIDE FLARE INSIDE PEDESTRIAN CIRCULATION PATH (SEE GENERAL NOTE 25)	5.0% (1:20) MIN. 9.5% (1:10.5) MAX.	10.0% (1:10) MAX.	
SIDE FLARE OUTSIDE PEDESTRIAN CIRCULATION PATH (SEE GENERAL NOTE 25)	5.0% (1:20) MIN. 25% (1:4) MAX.		

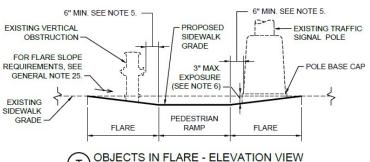
SIDE TREATMENT OPTIONS

- Shown for most cases
- Where side adjoins pedestrian circulation path - 9.5% Max side flare
- Where side adjoins nonwalkable area – 25% max side flare or sidewalk curb
- As directed by the engineer



CASE C1 AND C2 SIDE TREATMENT OPTIONS SCALE: N.T.S.





SCALE: N.T.S.



SIDE TREATMENT OPTIONS - EXAMPLES

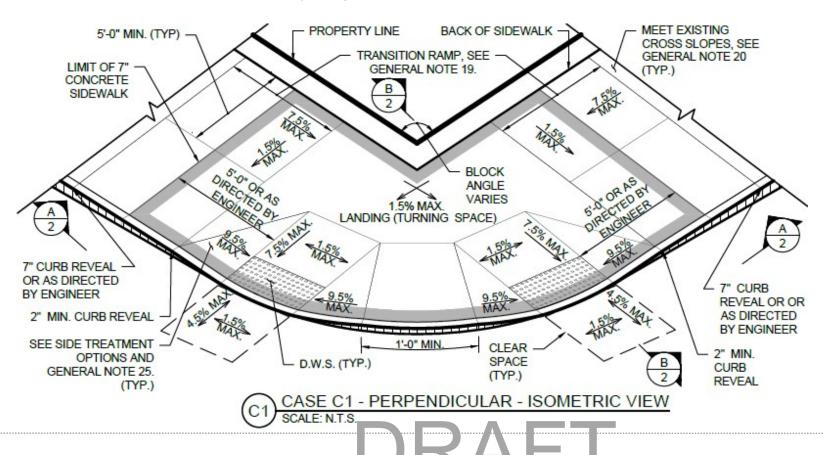






CASE C1 - PERPENDICULAR

- Evolved from previous Case I and Case III
- Pedestrian circulation path (PCP) 8 feet wide or greater
- Transition ramps as directed by engineer



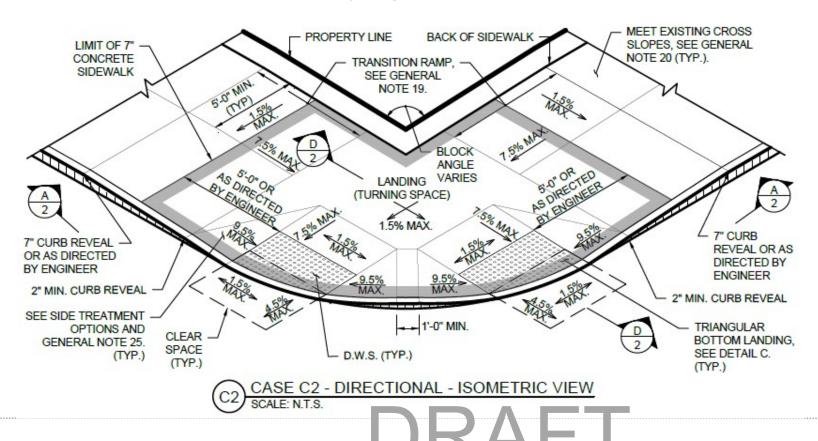
CASE C1 – PERPENDICULAR - EXAMPLES





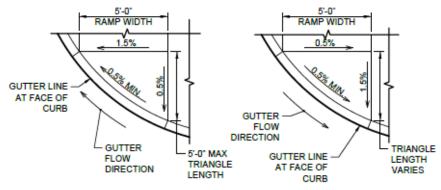
CASE C2 – DIRECTIONAL

- Similar to case C1, ramp alignment skewed from curb
- Bottom grade break must be perpendicular to ramp run
- Pedestrian circulation path 8'-0" wide or greater
- Transition ramps where directed by engineer

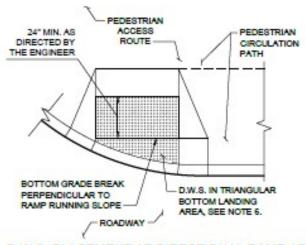


CASE C2 – DIRECTIONAL

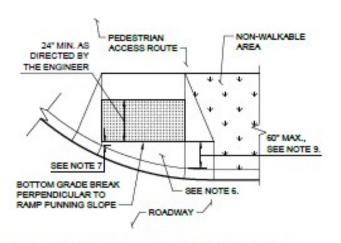
- Standard grading options provided
- D.W.S. as directed by engineer, typically required in triangular bottom landing
- Designer of record responsible for grading detail



TRIANGULAR BOTTOM LANDING GRADING DETAIL - PLAN SCALE: N.T.S.



D.W.S. PLACEMENT AT DIRECTIONAL RAMP ADJACENT
TO PEDESTRIAN CIRCULATION PATH. - PLAN DETAIL
SCALE: N.T.S.



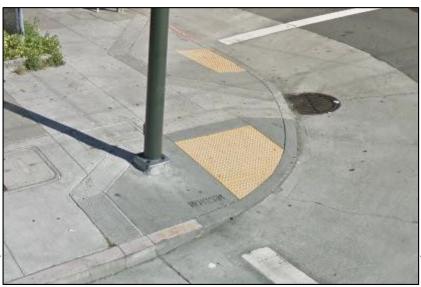
D.W.S. PLACEMENT AT DIRECTIONAL RAMP

ADJACENT TO NON-WALKABLE AREA - PLAN DETAIL

SCALE: N.T.S.

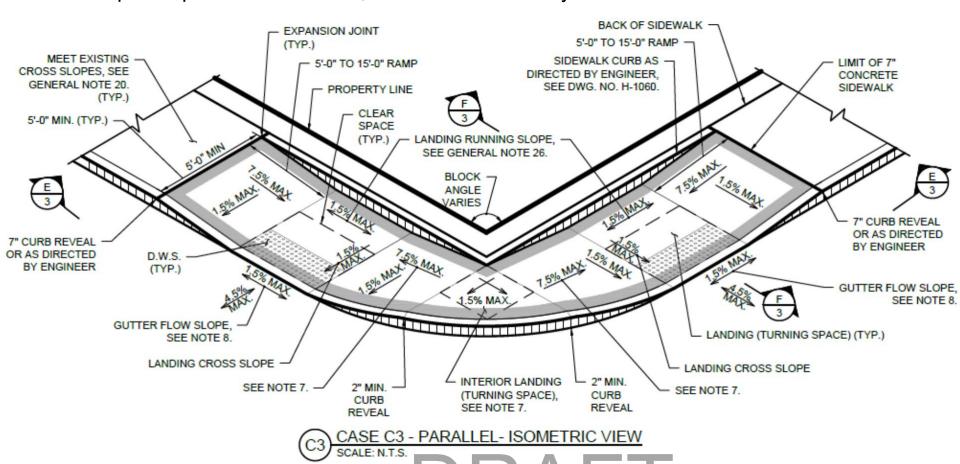
CASE C2 – DIRECTIONAL - EXAMPLES





CASE C3 - PARALLEL

- Pedestrian circulation path less than 8'-0" wide
- Ramps are parallel to curb line, no need to score any flares



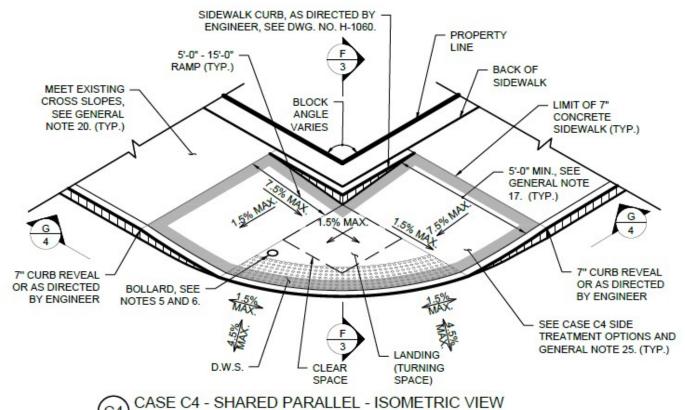
CASE C3 – PARALLEL - EXAMPLES





CASE C4 – SHARED PARALLEL

- Two parallel ramps with shared landing.
- Pedestrian circulation paths less than 8'-0" wide
- Requires DOT approval



CASE C4 – SHARED PARALLEL - EXAMPLES



SIDES



RIGHT RAMP



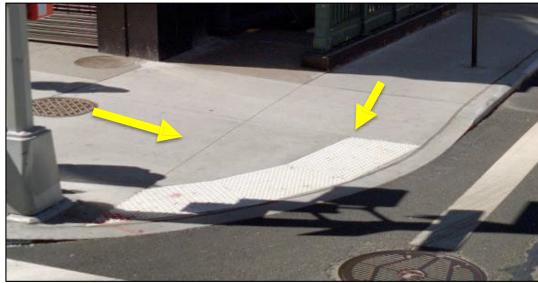


CURBED SIDES APPROACH

CASE C4 SIDE TREATMENT OPTIONS

SCALE: N.T.S.

RIGHT CURB

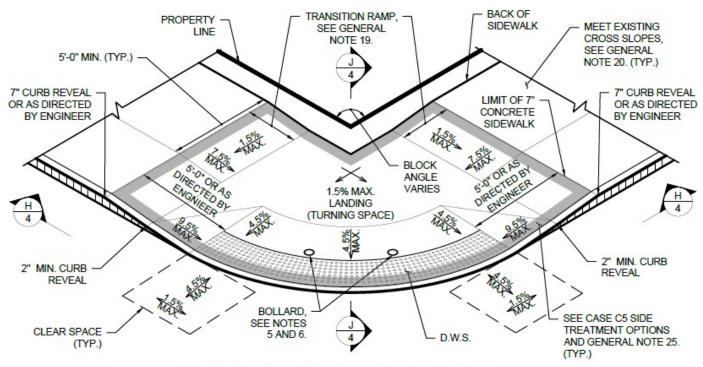




CASE C5 – BLENDED TRANSITION

- Replaces old Case II (Apex)
- Pedestrian circulation paths 8'-0" or greater
- 4.5% Max running slope (field layout)

- Minimum 8'-0" Wide
- Radial D.W.S.
- Requires DOT approval





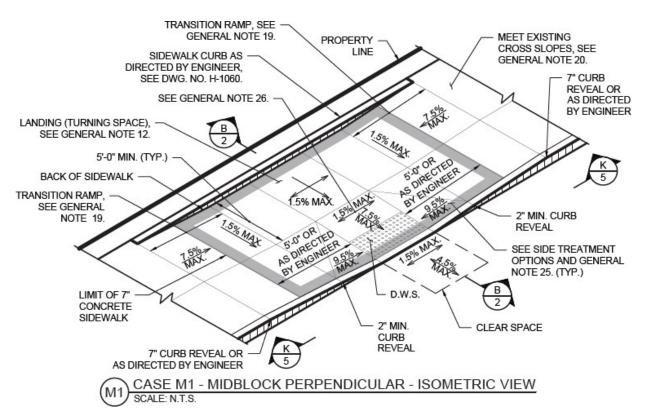
PERPENDICULAR CASES: C5 - EXAMPLE





CASE M1 – MIDBLOCK PERPENDICULAR

- New case, same concept as Case C1 Perpendicular
- Pedestrian circulation paths 8'-0" wide or greater



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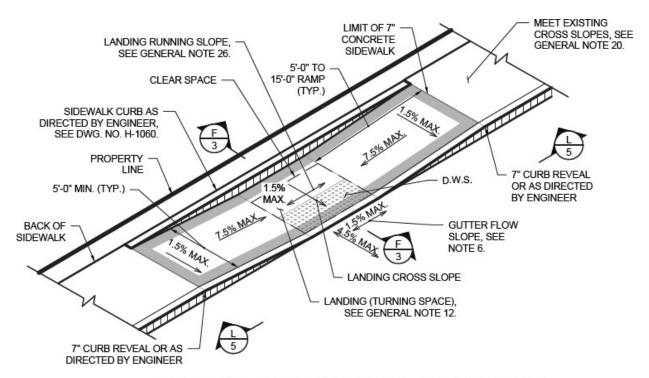
CASE M1 – MIDBLOCK PERPENDICULAR - EXAMPLES





CASE M2 – MIDBLOCK PARALLEL

- New case, same concept as Case C3 Parallel
- Pedestrian circulation paths less than 8'-0" wide



CASE M2 - MIDBLOCK PARALLEL - ISOMETRIC VIEW SCALE: N.T.S.

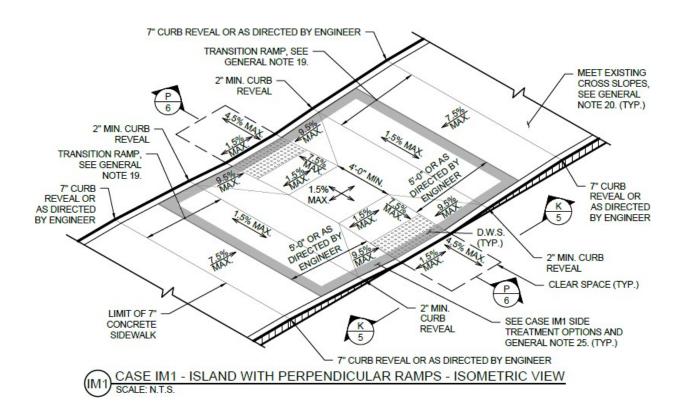
CASE M2 – MIDBLOCK PARALLEL - EXAMPLES





CASE IM1 – ISLAND WITH PERPENDICULAR RAMPS

Island widths less than 16'-0" require DOT Approval



CASE IM1 – ISLAND WITH PERPENDICULAR RAMPS - EXAMPLE

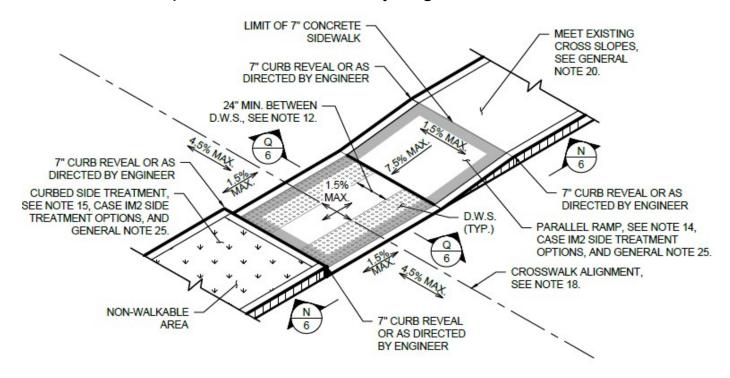


CASE IM2 – ISLAND CUT THROUGH

- Supersedes grading and geometry in H-1003
- Standardizes cut through width from TRF-02

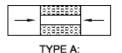
TABLE 2: CUT THROUGH WIDTHS			
CROSSWALK WIDTH	LESS THAN 14 FEET	14 FEET OR GREATER	
CUT THROUGH WIDTH	8 FEET	10 FEET	

• DWS must have 24" separation, as directed by engineer



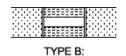


CASE IM2 – ISLAND CUT THROUGH - EXAMPLES

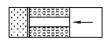


TWO PARALLEL

RAMPS



CURBED SIDES



TYPE C: ONE PARALLEL RAMP, ONE CURB

CASE IM2 SIDE TREATMENT OPTIONS SCALE: N.T.S.



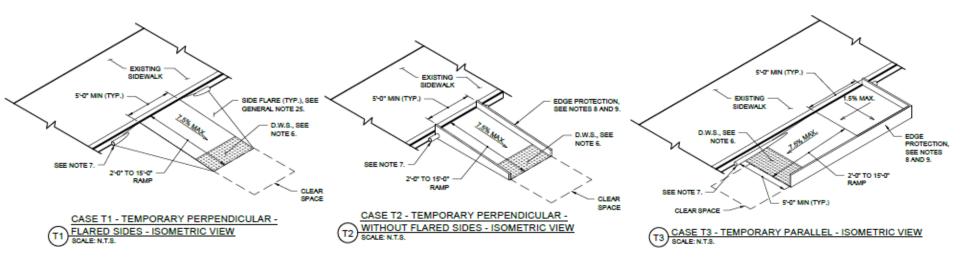


TYPE C

TYPE B

CASE T1, T2, T3 – TEMPORARY RAMPS

- Not a substitute for M.P.T. plans
- Drainage flow must be maintained, may require drainage pipe
- Material not specified; must be firm, stable, slip resistant, and fixed to ground



CASE T1, T2, T3 – TEMPORARY RAMPS - EXAMPLES

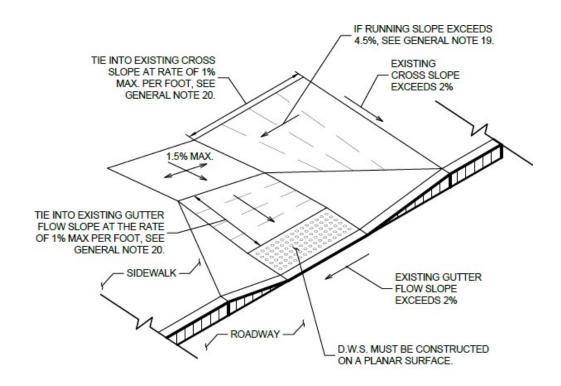






MEETING NON-COMPLIANT SLOPES

Guidance detail provided on sheet 8

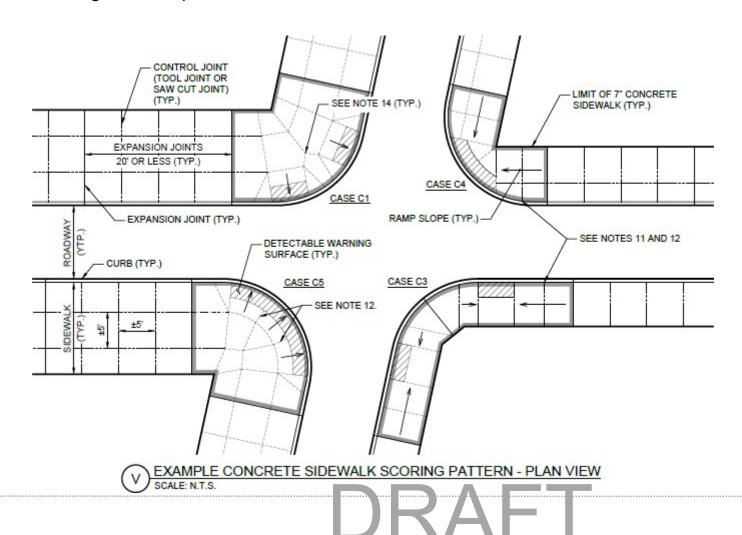






SAMPLE SCORING PATTERNS

Additional guidance provided on sheet 8



DETECTABLE WARNING SURFACES

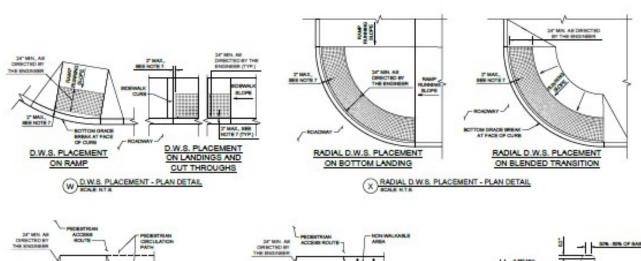
- Must meet requirements of sheet 9
- Radial D.W.S. is introduced

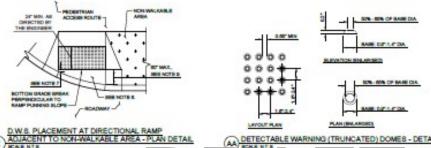
DOMESTIC TRANSPORT ARE

D.W.S. PLACEMENT AT DIRECTIONAL RAMP ADJACENT

OT RAILCHMANNER

- Requirements for D.W.S. adjacent to curb, 2" max offset
- D.W.S. must be installed per manufacturer's recommendations.





CTRE

FOR INDEX OF DRAWINGS, BLOPE LIMITS, LEGENC, GLOSSARY, GENERAL NOTES: 866 DWG, NO. H-1011-1.

DETECTABLE WARNING BURFACE (D.W.E.) NOTES:

- CWIS MUST BE INSTALLED AT ALL PLUSH CLIES LOCATIONS, WHERE THE PROSETTION CRICILATION PATH CRICKINGS A ROADWAY, RALLWAY, OR TRAFFIC COMPROLISE DEVIMENY.
- DWIE BLIEF BE PRIVALED ACROSS THE FULL WOTH OF FLUEN CURS, INCLUDING FULL PAMP WOTH, FULL SCHTCH LANGING WOTH, FULL BLIEFORD TRANSPICA WOTH, AND FULL CUT THROUGH WOTH SHAPES APPLICABLE;
- 4. DWIS MUST BE INSTALLED ACKORS THE PULL WIDTH OF THE PROBSTRUM CHICLARTON WITH, AT ANY STOP, YIRLD CONTROLLED, OR SERVALIDED DEVINANT CWIS MUST NOT BE INSTALLED AT UNCONTROLLED DEVINWAYYS.
- DWX MUST BE INSTALLED FOR A MINIBUM LENGTH OF IN INCIDES IN THE DIRECTION OF PROSERVAN TRUME. DWX. MUST BE INSTALLED OR OMITTED AT IE.AND AND REDIAN CLIT THROUGHER IN ACCORDANCE WITH NOTE 12 ON DWG. NO. HIGHLY, AND DIRECTION OF THE MINISHER.
- TO MANTAR DETCCHAILTY AT DISCHOOM, RURRE, DAYS, GARR BLEF, MOT BOOT SETMINER PRODRIENCH CHICALTON, ATTHE AND FLICK CHICK, WHICH THE BOO RURRE ALLONG A PROSETTION CHICALTON PATH, CAVE, MISST BE HOTELLED IT THE TRANSPOLAR BOTTOM LINGUIS AREA OF DISCHOOM, RURPE, WHIS THE SIZE FLARE ALLONG A ROMANULANDE AREA, DAYS, MAY BE OMITTION IT THE TRANSPOLAR BOTTOM LANDERS AREA OF DIRECTIONS, RURPE.
- WHERE PROPOSED AT THE BACK OF CLERK DAVIS MUST BE INSTALLED WITH A TWO INCH OT MAZEMUM OFFIRST FROM THE REPARKED A JOINT ON TOOLED RADIUS.
- B. DWK MUST BE INSTRUMED IN ACCORDANCE WITH THE MANUFACTURISTS RECOMMENDED PROCEDURES AS DIRECTED BY THE ENGINEER DWG MAY SE CUT OR TREMAND TO MIST THE REQUIREMENTS OF THIS DETAIL, AS DIRECTED AND APPROPRIENT THE RESIDENCE.
- IF THE TRANSLLAR ROTTON LANSING OF A DIRECTIONAL RAMP IS LONGER THAN 80 INCHES, THE DIVIS MUST BE INSTALLED IN THE SOTTOM LANSING AREA.
- THE DETAILS PROVIDED ARE NOT DRAWN TO SCAUS THE QUANTITY OF TRUNCATED DOMES DEPICTED ON THE DIVISION FOR ILLUSTRATION ONLY.
- DWS MUST PROVIDE COLOR CONTRAST WITH THE ALLONSED SCHMALK FOR DWS REQUIREMENTS INCLICING COLOR CONTRAST, SHE NYC DOT STANDARD HOMEN'S PRICE/CONDITION NO. 413 DB.
- 12. ON BLOPER OF FIVE PERCENT (MI) OR DIRECTOR, TRUNCATED DOMES MUST BE ALIGNED WITH THE LOWER GRADE BREAK OF THE RAME. ON BLOPES LIKES THAN FIVE PERCENT (MI), TRUNCATED DOMES DO NOT NEED TO SE ALIGNED WITH THE LOWER SHADE BREAK OF THE RAME.
- T3. DWS. MURT BE PROVIDED AT RAUROAD CROSSING IN ACCORDANCE WITH NEW YORK STATE AND PROSING, RAUROAD ADMINISTRATION REQUIREMENTS, DWS. LAYOUT A RAUROAD CROSSINGS MUST BE ASSISTED TO INVIDED FOR REVIEW AND APPROVIAL PRICK TO ITS CONSTRUCTION.
- MRREDGE DIME MUST BE INSTRUCED ON A FLAVAR SUPPLIED PRESENT WARRING, ANY CROSS BLOFF TRANSITIONS PARAMENCH WITHIN A SAMP OF TURBORS SPECIAL MUST BE SECULIARED OF THE DIMES, AT A MACRAIM RATE OF CRO-INCOME TO DRIVER LIBERAR SCOTT.
- 15. PRE-PARRICUTED RADIAL DIVIS. MAY 86 USED FOR RADIAL DIVIS. PLACEMENT WHITE PRODUCEMENT OF PRE-PARRICUTED RADIAL DIVIS. IS NOT PRACESS. RECTWOLLAR TES ARRIYS MAY 86 USED TO PROMODE RADIAL DIVIS. PLACEMENT AS DIRECTED BY THE ENGINEER. WHIRE USED, RADIAL DIVIS. OF ART TITTE MANY MEDITAL PAPILICASE REQUIREMENTS ON THIS SHEET, INCLUDED DOME PRACING.
- 16. DETECTABLE WARMING (TRUNCATED) DONES MUST MIST THE REQUIREMENTS OF
- WHERE AN EXISTING LITLIUTY CARTING BY LOCATED WITHIN THE PROPOSED LOCATION OF A DURS, THE CONTRACTION MAY GUT THE DURS TO ACCOMMOD TO THE UTILITY CARTING, SEE NOTE 5.

THANK YOU

Questions?









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